



City of Atlanta – Bureau of Planning **Martin Luther King, Jr. Drive Corridor Study**

Community Charette



January 18, 2005

Discussion Outline

- Background/Purpose of Study
- Overview of Existing Conditions
- Summary of Market Analysis
- Draft Goals and Objectives
- Issues and Challenges
- Community Charette (Break-out)
- Reporting of Charette Results
- Wrap-Up (Next Steps)





Background of Study

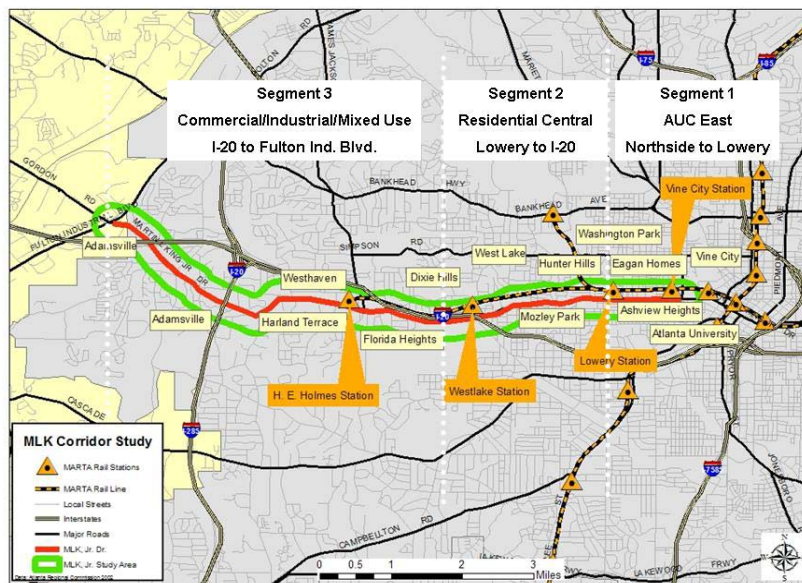
- Purpose is to Identify Strategies that
 - Provide Mobility Throughout the Entire Corridor
 - Incorporate a Full Range of Multi-Modal Transportation Options
 - Are Consistent with New Regional Transportation Initiatives (i.e., West Line Extension, Bus Rapid Transit, etc.)
 - Are Realistic, Feasible and Able to Implement



Background of Study

- Limits of the Corridor - *Northside Drive to Fulton Industrial Boulevard -1/4 Mile on Each Side of Corridor)*
- Breakdown into 3 Segments/Character Areas (*see map*)





Background of Study

- Coordination with and Consideration of the Goals and Recommendations of Other Plans and Studies in the Corridor (*H.E. Holmes Study, West Side Village, Vine City Plan, etc.*)
- Potential for LCI Funding
- Coordination with Regional Agencies (ARC, GRTA, etc) Throughout Study Process



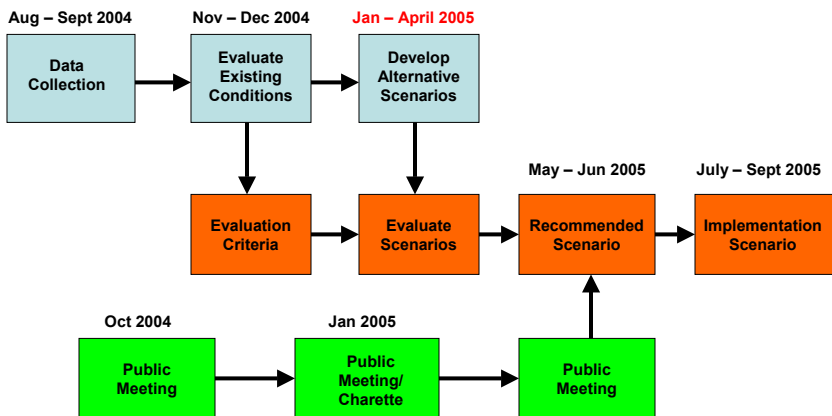
Background of Study

The Study Process has consisted of:

- August – September 2004
 - Collection of Data & Base Information
 - Interview of Stakeholders
 - Establish an Advisory Committee
- October 2004 – November 2004
 - Collection of Data & Base Information
 - Advisory Committee Meetings
 - 1st Public Meeting (Kick-off Meeting)
- December 2004 - Present
 - Evaluate Existing Transportation & Land Use Facilities
 - Advisory Committee Meetings
 - Preparation for Community Charette



Study Process



Overview of Existing Conditions

Demographics:

- Population: 2.41% Increase from '90-'00; 60,912 in the Study Area
- Age: Almost 30% are between 25-44 in the Study Area (Average Age is 35)
- Race: White - .87%, Black – 97.86%, Asian - .19%, Other Races – 1.08%
- Households: 2.99% Increase Over Past Decade, 22,248 in the Study Area, Average Household Size – 2.6
- Income: Average - \$35,404; Median - \$26,767; Per Capita - \$13,501
- Employment: 28,301 Total Employees – Majority are in Manufacturing (6,070), Wholesale (5,614) or Services (6,944) Sectors



Existing Conditions - Conclusions

Demographics

- Population: Due to moderate growth rates (2.49%), there are only 64,023 people and 22,860 households forecasted by 2010 (compared with 3.25% for the City and 10.64% for the MSA)
- Age: Future forecasts indicate that the corridor will have more youth (under 21) than both the City and the MSA as well as more elderly (60 & older). These two groups and their impacts need to be considered for any future development (commercial, housing, etc.)
- Race: The corridor has been mostly Black (98%) and will continue that trend in the future. Any future development plans (commercial, retail, etc.) should consider the cultural aspect of the community.
- Housing: The corridor is mostly residential with diverse intensities (single, multi, mixed use). Based on the population projections, there should be a range of housing options for low income populations (young adults and elderly)
- Employment/Income: The majority of jobs are in manufacturing and service sectors, which is consistent with the average income (\$35,404) in the corridor. There should be an emphasis to diversify the employment opportunities to be consistent with housing and retail options and that will facilitate income potential.



Overview of Existing Conditions – Market Analysis

What is a Market Analysis?

- The study of economic, demographic and other factors made to determine supply and demand, market trends and other factors important to a specific geographic area.



Overview of Existing Conditions – Market Analysis

- Structure of Martin Luther King Market Analysis

RESIDENTIAL
COMMERICAL



MARTA Transit Oriented
Development
&
University-based development

INDUSTRIAL



Fulton Industrial Blvd.



Overview of Existing Conditions – Market Analysis

Demographic Summary of MLK Study Area

- Employment - increase 25% by 2030
- In 2004, 30% of the households had an annual income of less than \$15,000.
- Median household income in 2004 was \$26,767
 - 32% lower than the city of Atlanta at \$39,550
 - 54% lower than the Atlanta MSA at \$58,250



Overview of Existing Conditions – Market Analysis

Market Analysis Summary:

- Potential Market Demand for Owner-Occupied Housing - 569 units annually
- Potential Market Demand for Renter-Occupied Housing - 1,800 units annually
- The Median Owner-Occupied Housing Value - \$93,833
- The Median Effective Buying Income per household - \$23,471 annually



Overview of Existing Conditions

Transportation:

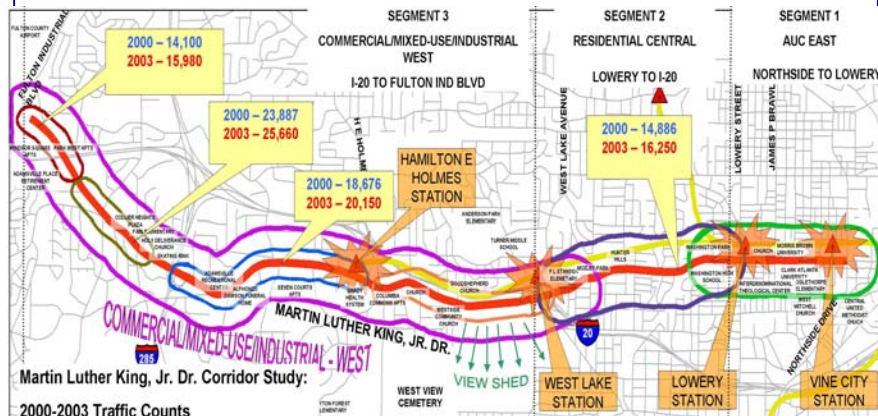
- Heavily Utilized East-West Corridor (Alternate to I-20)
- Provides Access to Major Activity Centers in the City
- Home to 5 MARTA Stations & Future Rail Expansion
- Corridor is in the ARC's Congestion Management System – Ranks 57 out of 73 Corridors in the Atlanta Region
- Majority of the Corridor (from I-285 East to downtown) is Classified as Congested
- Majority of the Corridor is on State Highway System (SR 139)
- 22 MARTA Bus Routes along the Corridor
- Corridor Connects to Major Industrial Sector (Fulton Industrial Blvd.), which is a Major Employment Center



Overview of Existing Conditions

Transportation:

- Traffic Count Information: Annual Average Daily Traffic (AADT) Counts for 2000 & 2003



Overview of Existing Conditions

Transportation:

- Medium Conditions for Bicycling (ARC Sufficiency Ratings)
- Significant Accident Rate
- Standard Sidewalk, Curb, Gutter and Some Streetscape Elements along the Eastern Portion of Corridor (AUC/Historic Westside Village)
- Heavy Pedestrian Use Throughout Corridor
- Substandard Sidewalk in Some Areas
- Unsafe Pedestrian Accessibility at Some Intersections
- According to ARC – Too Many Driveways, Poor Intersection Geometry and Heavy Peak Period Volumes
- Need for Traffic Calming Measures



Existing Conditions - Conclusions

Transportation:

- Due to future proposed MARTA expansion, there needs to be an emphasis on transit-oriented development
- Due to heavy peak period traffic volumes, there should be a concerted effort to accommodate a full range of multi-modal options
- Based on the high pedestrian activity, there needs to be a focus on ensuring safe pedestrian access to the many activity centers along the corridor
- Based on the large number of driveways/curb cuts and poor intersection geometry, access management measures are needed for an efficient flow of traffic to/from Fulton Industrial Blvd.
- Due to speeding and accident rates, there needs to be some traffic calming devices along the corridor
- Extensive coordination with GDOT & regional agencies is required



Overview of Existing Conditions

Land Use:

- Due to the Extreme Differences in Character, Type of Development and Land Uses, the Corridor is Divided into 3 Distinct Segments with a unique theme:
 - Atlanta University Center/Educational/East (Northside Drive to Lowery Blvd.)
 - Stable Residential/Central (Lowery Blvd. to I-20)
 - Mixed-Use/West (I-20 to Fulton Industrial Blvd.)
- Predominate Land Use is Residential
- Some Historic Single-Family Neighborhoods
- Dense/Mixed Use Patterns
- Transit Oriented Development (TOD)
- Some Community Commercial/Retail
- Neighborhood Parks & Schools



Overview of Existing Conditions

Land Use:

- Ribbon/Strip Commercial & Retail Development
- Opportunities for Infill Development
- Vacant Unkempt Lots
- Some Transitional Housing
- Good Potential for Revitalization
- Zoning Designations are Inconsistent with Existing Patterns of Development



Existing Conditions - Conclusions

Land Use:

- Due to the historic aspect of the corridor, existing neighborhoods, institutions and cultural resources should be preserved and protected
- To facilitate access management measures, there needs to be a reduction of ribbon/strip commercial sites & retail uses
- There should be an emphasis on providing and preserving land uses that are consistent with existing neighborhoods and also support and enhance public parks and open space
- Based on the transit-oriented character of the corridor, there should be an increase in dense/mixed use patterns, particularly in activity centers
- There are additional opportunities for infill development due to a significant amount of vacant unkempt lots
- Zoning designations along the corridor need to be consistent with the actual development patterns



Existing Conditions

Urban Design:

- Good Potential for Revitalization
- Minimal Streetscape Elements
- Opportunity for Gateways/Markers
- No Evidence of a Theme for Corridor
- Community Blight/Trash
- Potential for Façade Improvements
- Lack of Signage and Community Identifiers
- Lack of Plantings and Landscape Enhancements



Existing Conditions - Conclusions

Urban Design:

- To fully enhance the neighborhoods, there should be more landscape and hardscape enhancements in appropriate areas
- For an aesthetic improvement, there needs to be improved pedestrian lighting, street trees and furniture in appropriate areas
- To enhance the entryways of the corridor, a gateway element should be considered for the corridor
- To preserve the character of the diverse communities, there needs to be a designation of a theme for appropriate areas along the corridor
- Due to a lack of signage and identifiers, there is a need for kiosks, signage, fountains etc. to make the corridor more user-friendly for commuters, residents and visitors
- There are good opportunities for a vibrant pedestrian-oriented streetscape environment



Draft Goals & Objectives


- Promote Safety and Security for an Enhanced Quality of Life
- Develop an Appropriate Mix of Land Use and Zoning to Meet Community Needs
 - Moderate the Spread of Strip Commercial Development
 - Encourage Mixed-Use Development around MARTA Stations
 - Have Certain Parcels Properly Zoned for Future Development/Redevelopment
- Improve the Existing Transportation Issues/Concerns
 - Coordinate with Recommendations of Other Corridor Plans/Studies
 - Increase the Number of Multi-Modal Options
 - Enhance Transit Accessibility, Service and Options
 - Improve Transportation Safety and Congestion at Intersections
 - Address Public Infrastructure Concerns





Draft Goals & Objectives

- Create Maintain a Stable Economic Base
 - Establish Regulations for Affordable Housing in Appropriate Areas
- Preserve and Enhance the Historic Residential & Commercial Areas
 - Diverse the Business and Mix and Target Quality Employers
 - Minimize Impacts to Historic/Cultural Sites
 - Ensure the Availability of Cultural & Recreational Opportunities
- Develop Strategies Consistent with Regional Agencies
- Protect & Preserve Environmentally Sensitive and Greenspace/Conservation Areas
 - Minimize Impacts to Environmentally Sensitive Areas



Major Issues/Needs

Corridor Issues From Public Input & Data Collection:

- Safety/Crime
- Maintenance of City Services
- Parking and Traffic Issues
- Community Blight/Trash
- Lack of Diverse Retail
- Improved Urban Design/Development Standards
- Speeding/Pedestrian Safety
- Economic Development
- No Access Management
- Trees & Parks
- No Landmarks or Gateways
- No Quality Commercial/Office Development
- Aesthetics/Landscaping



Community Charette/Breakout

Instructions:

- Join a work station
- Per facilitator instructions, discuss problems/improvements needed along the entire corridor in each of the four planning areas: Transportation, Land Use, Urban Design or Housing.
- Collaboratively develop a list of potential solutions to the problems and needs for corridor with other members of the community (again, within either Transportation, Land Use, Urban Design or Housing.
- Identify, from the visual aids and tool kits provided, a list of potential solutions to the problems.
- Place each solution on the corridor map and write comments explaining how, where and why the solution should be implemented.
- After identifying solutions, designate a speaker that will summarize major solutions identified by the group.



Community Charette Session – Guidelines

The Rules for the Session are as follows:

- Actively participate in the discussion at least one of the groups
- Identify “Likes” and “Dislikes”
- Focus on transportation, land use, urban design and housing
- Everybody gets a chance to be heard
- Respect each other-don’t talk while others are talking
- Provide additional comments on blank comment form
- Have a fun, productive dialogue!!!!





Community Charette Session

Reporting of Results



Wrap-Up

Next Steps

- Continue to Coordinate with Advisory Committee & Stakeholders
- Continue to Evaluate Existing Conditions and Gather Additional Data
- Develop Alternative Scenarios for Corridor
- Meeting with ARC & GDOT to Assess LCI Study Criteria & Guidelines for Study
- Post Displays & Correspondence on City Web Site
- Prepare for Upcoming Public Meetings (TBD)

